

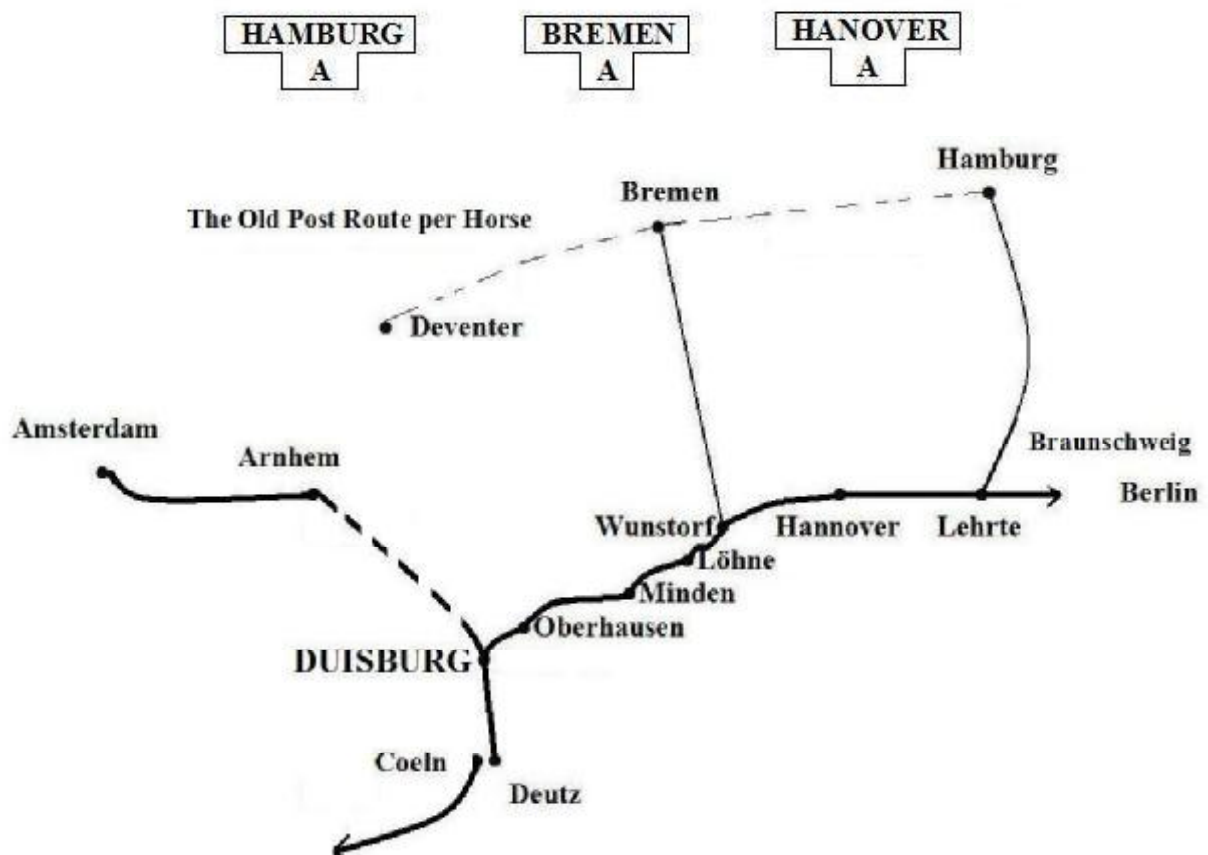
## The impact of the German Railways on the Post in the Netherlands 1848-1856

Transport of mail by railways was very attractive in those days because of the speed. Here are some examples of various forms of conveyance:

- A postman by foot 5 Km/hour
- A horseman maximum 10 Km/hour
- Steamship 20 Km/hour
- The train 40 Km/hour

The Netherlands circular Nr 381 of Apr.1<sup>st</sup> 1848 announced some changes, without mentioning the word of “Railway“:

- The main part of the mail to and from Hamburg, Bremen and the Kingdom of Hanover should *now* pass Arnhem instead of Deventer.
- Special Netherlands hand-stamps were introduced to mark the mail coming in via Arnhem. This is the so-called “Hammer” or “Korte-T” hand-stamps. “A” for Arnhem



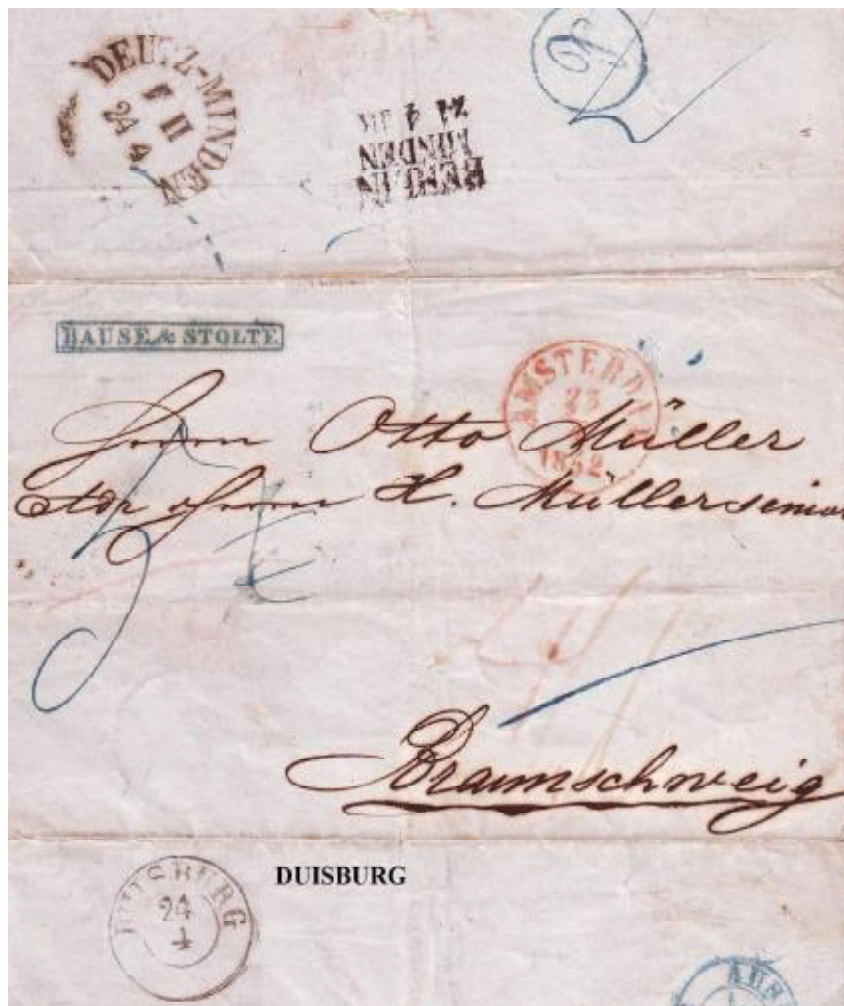
**Figure 1.** We see the situation in 1848 for railways near the Netherlands/Prussian border. The old horse-route Hamburg-Deventer was replaced by a railway 150 Km to the south



**Figure 2** Aarhus (DK) 1853 to Arnhem with the HAMBURG/A hand-stamp to indicate the new entry via Arnhem (Since April 1848)

**The next change came April 3<sup>rd</sup> 1851 by the NL circular Nr 437**

- The post-office of Arnhem should make up sealed bundles and direct them to the Train Post-Offices at the Minden-Deutz railway.
- The German office of Duisburg, which was a station on the railway was assigned to receive the mail from Arnhem (and a good guess would be that the office in Duisburg corresponded with the train)



**Figure 3.** Amsterdam 1852 to Braunschweig marked by a **DUISBURG** hand-stamp.

I have only seen one letter with the DUISBURG hand-stamp applied as border-mark

**Circular nr 485 of Dec 23<sup>rd</sup> 1853 reported:**

Arnhem should now correspond with Oberhausen and no longer with Duisburg. Post-marks from Oberhausen are rather common. This change may be inspired from contract closed in 1851 between the Netherlands and Prussia about a railway between Arnhem and Oberhausen.



**Figure 4.** Düsseldorf 1855 to Schiedam. Transit mark from Oberhausen Bahnhof

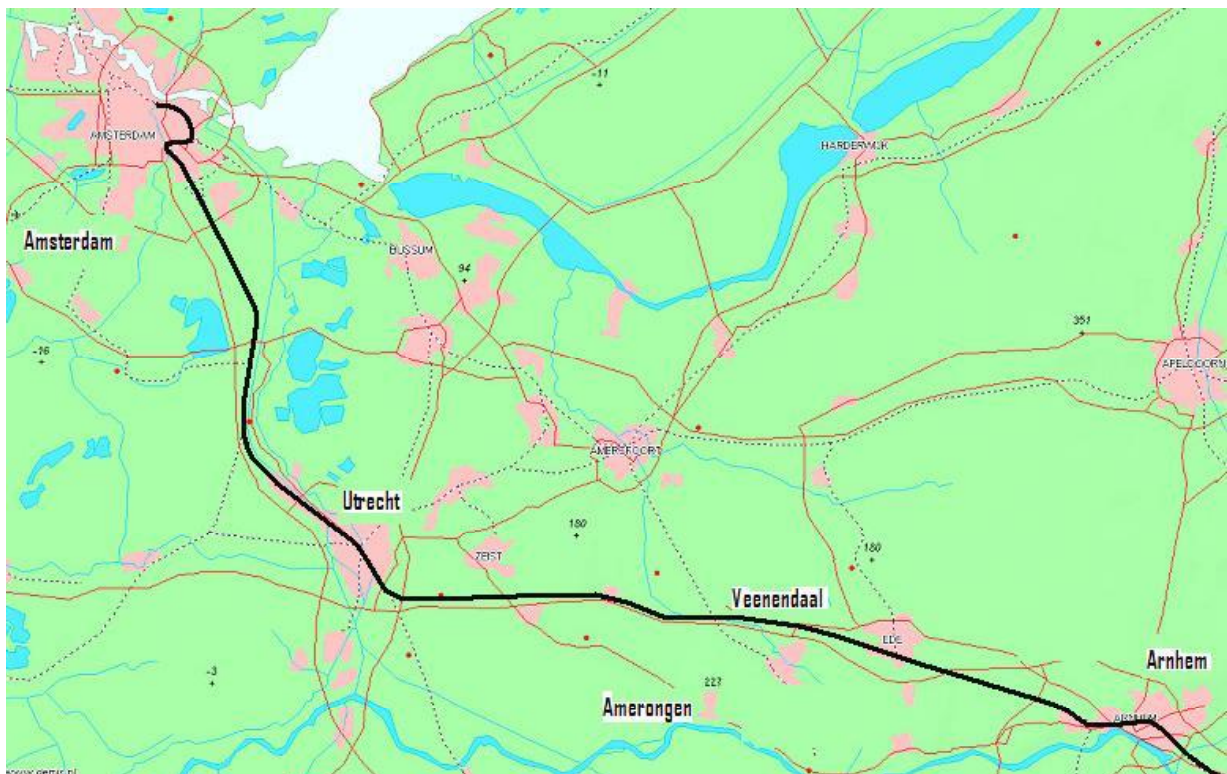
October 1856 Arnhem was connected by rails to Oberhausen and a new era could begin.

Finally we shall see a domestic Netherlands letter sent by the Amsterdam-Arnhem railway (The Neder Rijn Spoorweg)

To be continued on the next page.....



**Figure 5.** Amerongen 1863 to Arnhem conveyed by carriage and horse to the station of Veenendaal and transferred to the train to Arnhem. Postage due 5 cents.



**Figure 6.** Map for the letter in Figure 5

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The End Oct 2011